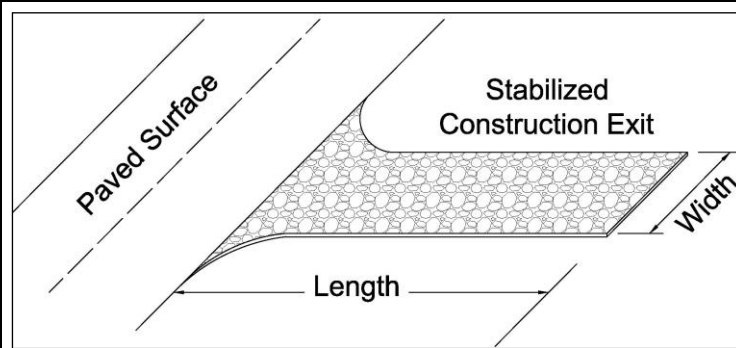


### 3.11 Stabilized Construction Exit

Sediment Control



**Description:** A stabilized construction exit is a pad of crushed stone, recycled concrete or other rock material placed on geotextile filter cloth to dislodge soil and other debris from construction equipment and vehicle tires prior to exiting the construction site. The object is to minimize the tracking of soil onto public roadways where it will be suspended by stormwater runoff.

**KEY CONSIDERATIONS**

**DESIGN CRITERIA:**

- Slope exit away from offsite paved surface
- Minimum width and length dependent on size of disturbed area, which correlates to traffic volume
- 6 inches minimum thickness of stone layer
- Stone of 3 to 5 inches in size
- Add a wheel cleaning system when inspections reveal the stabilized exit does not prevent tracking

**ADVANTAGES / BENEFITS:**

- Reduces tracking of soil onto public streets
- Directs traffic to a controlled access point
- Protects other sediment controls by limiting the area disturbed

**DISADVANTAGES / LIMITATIONS:**

- Effectiveness dependent on limiting ingress and egress to the stabilized exit
- A wheel washing system may also be required to remove clay soil from tires, particularly in wet conditions

**MAINTENANCE REQUIREMENTS:**

- Inspect regularly
- Replace rock when sediment in the void area between the rocks is visible on the surface
- Periodically re-grade and top dress with additional stone to maintain efficiency

**APPLICATIONS**

- Perimeter Control
- Slope Protection
- Sediment Barrier
- Channel Protection
- Temporary Stabilization**
- Final Stabilization
- Waste Management
- Housekeeping Practices

**Fe=N/A**

**IMPLEMENTATION CONSIDERATIONS**

- Capital Costs
- Maintenance
- Training
- Suitability for Slopes > 5%

**Other Considerations:**

- None

**TARGETED POLLUTANTS**

- Sediment
- Nutrients & Toxic Materials
- Oil & Grease
- Floatable Materials
- Other Construction Wastes

### 3.11.1 Primary Use

Stabilized construction exits are used to remove soil, mud and other matter from vehicles that drive off of a construction site onto public streets. Stabilized exits reduce the need to remove sediment from streets. When used properly, they also control traffic by directing vehicles a single (or two for larger sites) location. Controlling traffic onto and off of the site reduces the number and quantity of disturbed areas and provides protection for other sediment controls by decreasing the potential for vehicles to drive over the control.

### 3.11.2 Applications

Stabilized construction exits are used on all construction sites with a disturbed area of one acre or larger and are a recommended practice for smaller construction sites. A stabilized exit is used on individual residential lots until the driveway is placed. Stabilized construction exits may be used in conjunction with wheel cleaning systems as described in [Section 3.16 Wheel Cleaning Systems](#).

### 3.11.3 Design Criteria

- Limit site access to one route during construction, if possible; two routes for linear and larger projects.
- Prevent traffic from avoiding or shortcutting the full length of the construction exit by installing barriers. Barriers may consist of silt fence, construction safety fencing, or similar barriers.
- Design the access point(s) to be at the upslope side of the construction site. Do not place construction access at the lowest point on the construction site.
- Stabilized construction exits are to be constructed such that drainage across the exit is directed to a controlled, stabilized outlet onsite with provisions for storage, proper filtration, and removal of wash water.
- The exit must be sloped away from the paved surface so that stormwater from the site does not discharge through the exit onto roadways.
- Minimum width of exit shall be 15 feet.
- The construction exit material shall be a minimum thickness of 6 inches. The stone or recycled concrete used shall be 3 to 5 inches in size with little or no fines.
- The geotextile fabric must meet the following minimum criteria:
  - Tensile Strength, ASTM D4632 Test Method for Grab Breaking Load and Elongation of Geotextiles, 300 lbs.
  - Puncture Strength, ASTM D4833 Test Method for Index Puncture Resistance of Geotextiles, Geomembranes, and Related Products, 120 lbs.
  - Mullen Burst Rating, ASTM D3786 Standard Test Method for Hydraulic Bursting Strength of Textile Fabrics-Diaphragm Bursting Strength Tester Method, 600 psi.
  - Apparent Opening Size, ASTM D4751 Test Method for Determining Apparent Opening Size of a Geotextile, U.S. Sieve No. 40 (max).
- Rock by itself may not be sufficient to remove clay soils from wheels, particularly in wet conditions. When necessary, vehicles must be cleaned to remove sediment prior to entering paved roads, streets, or parking lots. Refer to [Section 3.16 Wheel Cleaning Systems](#) for additional controls.
- Using water to wash sediment from streets is prohibited
- Minimum dimensions for the stabilized exit shall be as follows:

| <b><i>Disturbed Area</i></b> | <b><i>Min. Width of Exit</i></b> | <b><i>Min. Length of Exit</i></b> |
|------------------------------|----------------------------------|-----------------------------------|
| < 1 Acre                     | 15 feet                          | 20 feet                           |
| ≥ 1 Acre but < 5 Acres       | 25 feet                          | 50 feet                           |
| ≥ 5 Acres                    | 30 feet                          | 50 feet                           |

- If a wheel cleaning system is used, the width of the stabilized exit may be reduced to funnel traffic into the system. Refer to [Section 3.16 Wheel Cleaning](#).

### ***3.11.4 Design Guidance and Specifications***

Specifications for construction of this item may be found in the Standard Specifications for Public Works Construction – North Central Texas Council of Governments, Section 201.10 Stabilized Construction Entrance and in the Standard Specifications for Construction and Maintenance of Highways, Streets and Bridges (TxDOT 2004) Item 506.2.E and Item 506.4.C.5.

### ***3.11.5 Inspection and Maintenance Requirements***

Construction exits should be inspected regularly (at least as often as required by the TPDES Construction General Permit). The stabilized construction exit shall be maintained in a condition that prevents tracking or flow of sediment onto paved surfaces. Periodic re-grading and top dressing with additional stone must be done to keep the efficiency of the exit from diminishing. The rock shall be re-graded when ruts appear. Additional rock shall be added when soil is showing through the rock surface.

Additional controls are needed if inspections reveal a properly installed and maintained exit, but tracking of soil outside the construction area is still evident. Additional controls may be daily sweeping of all soil spilled, dropped, or tracked onto public rights-of-way or the installation of a wheel cleaning system.

### ***3.11.6 Example Schematics***

The following schematics are example applications of the construction control. They are intended to assist in understanding the control's design and function.

The schematics are **not for construction**. They may serve as a starting point for creating a construction detail, but they must be site adapted by the designer. In addition, dimensions and notes appropriate for the application must be added by the designer.

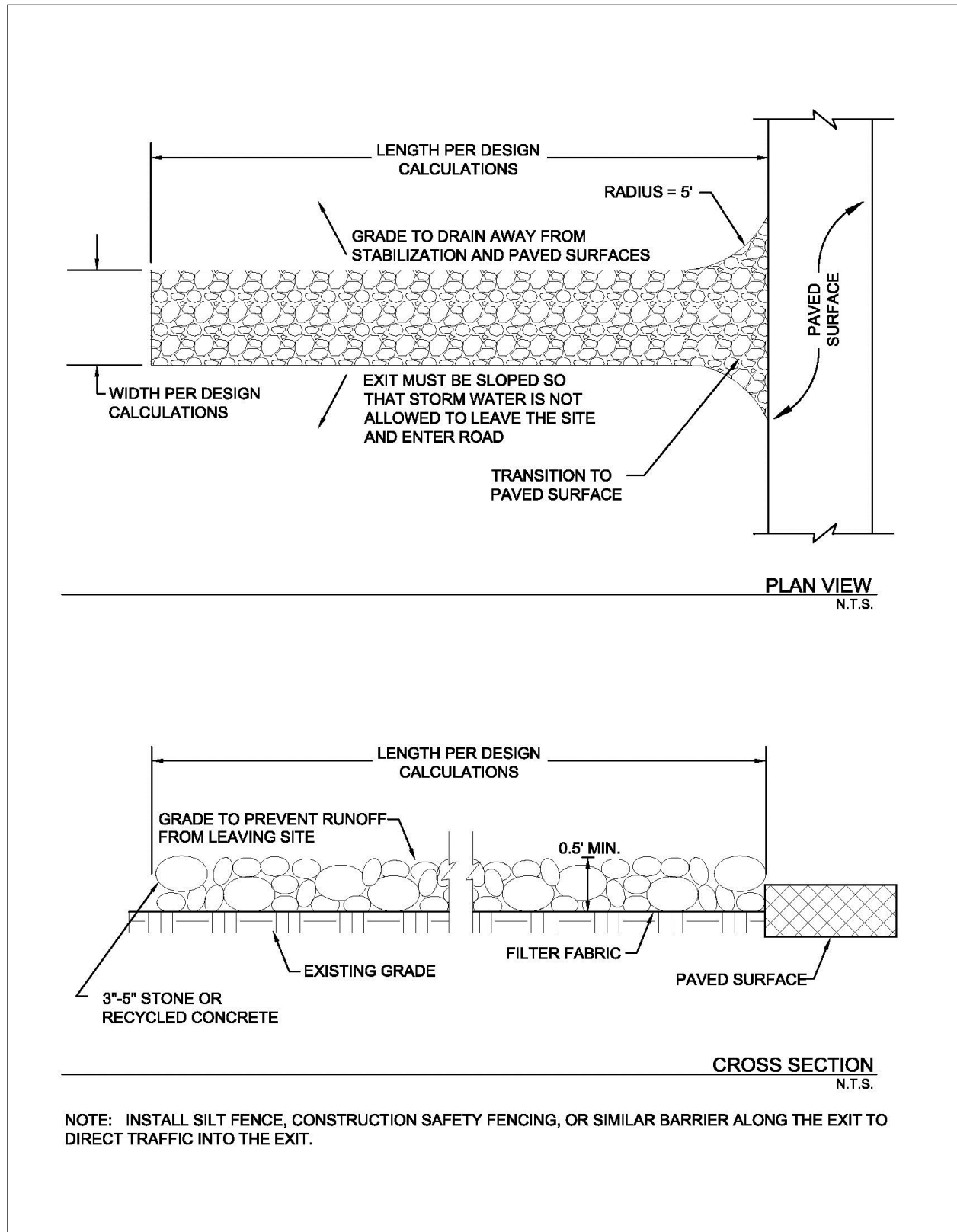


Figure 3.29 Schematics of Stabilized Construction Exit